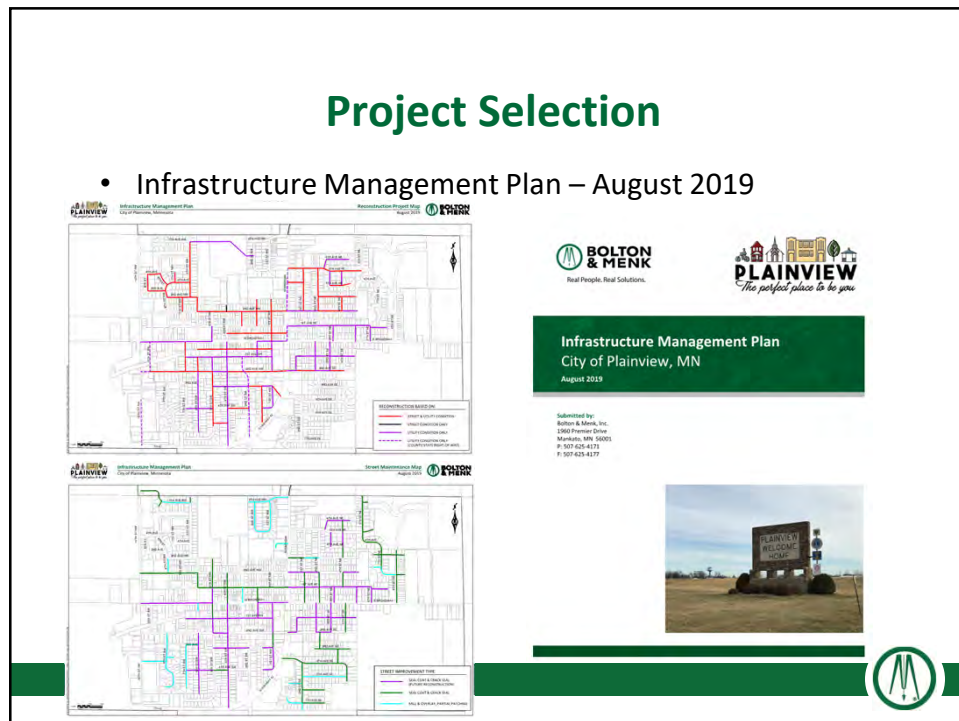




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2

Project Location



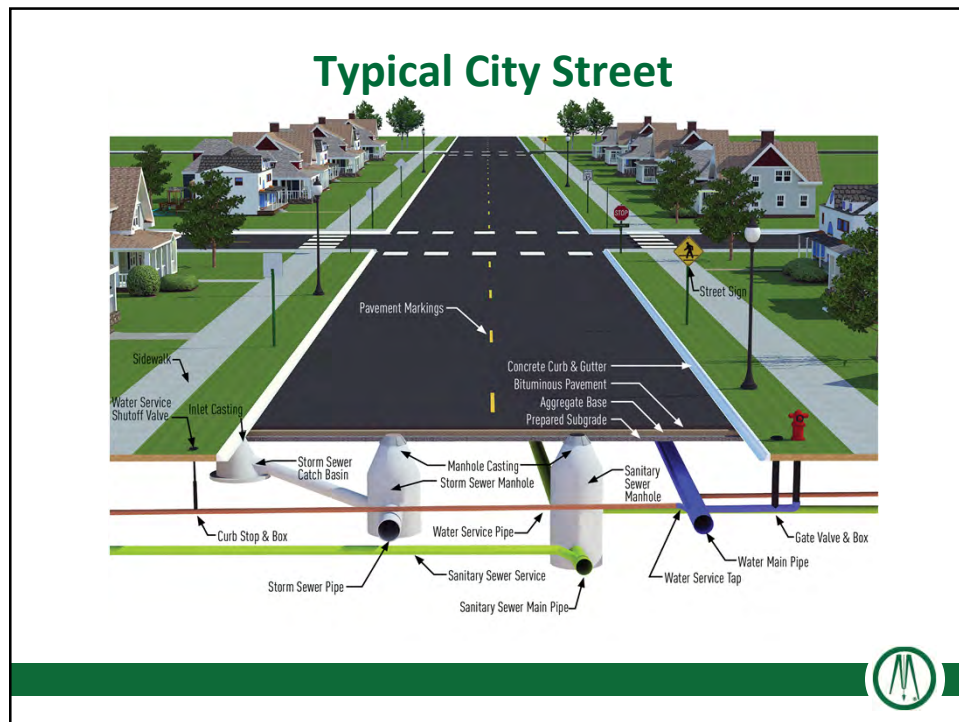
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Agenda

- Project Area
- Existing Conditions
- Proposed Improvements
- Cost Estimate & Financing
- Schedule & Next Steps
- Questions



4



5

Existing Conditions Streets

- Hillcrest Second Subdivision
 - 36-ft wide street
 - 60-ft wide right-of-way
- 2nd Street Southwest
 - 36-ft wide street
 - 60-ft wide right-of-way
- Asphalt surface – poor condition
- Concrete curb and gutter – fair to poor condition
- Miscellaneous streets will be overlaid as a maintenance effort to prolong pavement life

6



Exhibit 1 – Street & Curb Condition, Hillcrest Second Addition



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Exhibit 2 – Street Condition, Hillcrest Second Addition



8



Exhibit 3 – Pavement Condition, 2nd Street Southwest



9



Exhibit 4 – Pavement Condition, 2nd Street Southwest



10

Existing Conditions Storm Sewer

Hillcrest Second Addition

- Existing 12-inch and 15-inch reinforced concrete pipe
- Drainage discharges into the 10th Street Northwest ditch
- System is undersized – needs more inlets
- The catch basins at the intersection of 8th Street Northwest and 3rd Avenue Northwest are severely deteriorated and need to be replaced.

2nd Street Southwest

- Existing 15-inch reinforced concrete pipe to the North
- There is a highpoint approximately at 2nd Street Southwest and 4th Avenue Southwest where water drains north or south. Water draining south ultimately discharges into MnDOT (TH 42) right-of-way.



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Existing Conditions Sanitary Sewer

- Systems are approximately 50 years old
- 8-inch vitrified clay pipe
- Mineral deposits, offset joints, pipe sags, broken and damaged pipe, and blockages
- Manholes with evidence of infiltration
- Infiltration – increases flows, increases backup and bypass risk, increases pumping costs
- Shallow depths – Approximately 6-feet deep at the South end of 2nd Street SW



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Existing Conditions Watermain

- Systems are approximately 50 years old
- 4-inch, 6-inch, and 8-inch cast iron pipe (CIP)
- CIP of this age is typically brittle, corroded, susceptible to breaks, and reduced in diameter due to mineral buildup
- History of watermain breaks along 2nd Street SW
- 4-inch pipe is undersized for fire protection



13

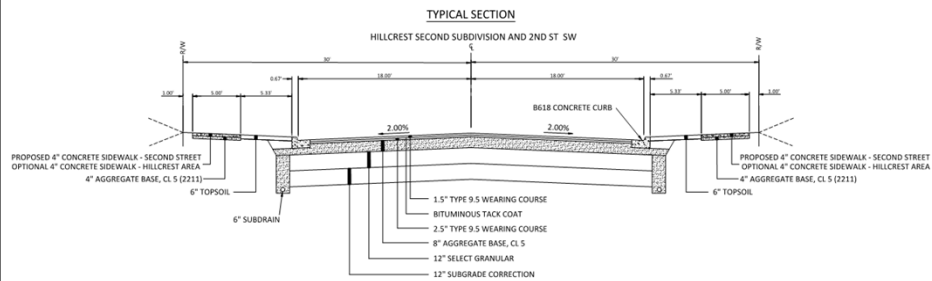
Proposed Conditions Streets

- 36-ft wide – allows for two 10-ft traffic lanes and 8-ft parking lanes on both sides (standard residential street width)
- Asphalt surfacing, concrete curb and gutter, concrete driveway aprons
- 5-ft concrete sidewalks with ADA pedestrian ramps on 2nd Street SW
- OPTIONAL 5-ft sidewalks for the Hillcrest Second Addition



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Proposed Conditions Streets



15

Proposed Conditions Storm Sewer



- New 12-inch to 15-inch reinforced concrete pipe
- Additional inlets to take water faster
- Larger inlet grates
- 6-inch perforated subdrain with sump pump service stubs

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Proposed Conditions Sanitary Sewer & Services



- 8" Diameter PVC Pipe
- Concrete Manholes
- 4" Services to Property Line
- Watertight System
- Shallow depths will remain
- Insulation provided for pipe and services with less than 7-ft of cover



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Proposed Conditions Watermain & Services

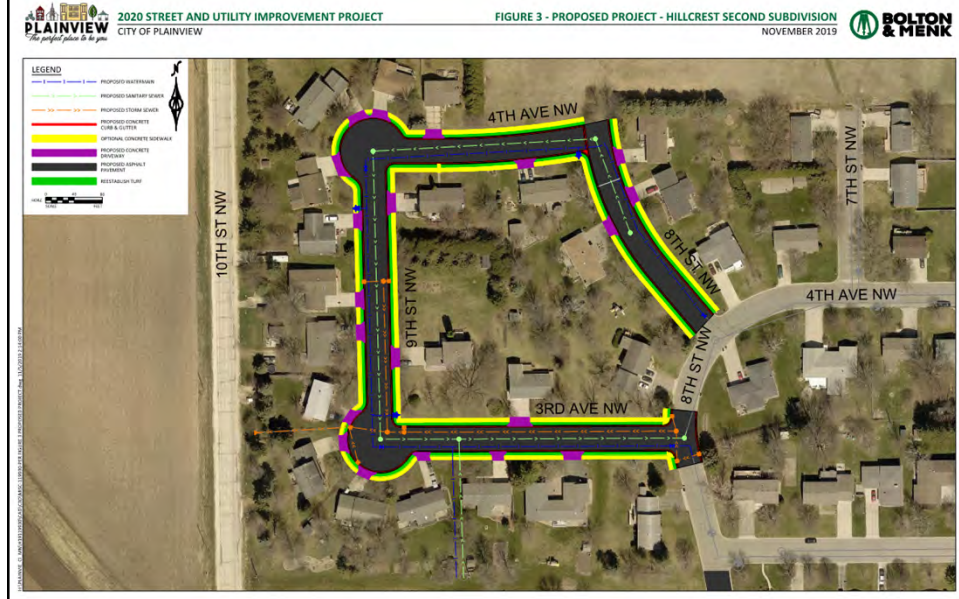


- 8" Diameter PVC
- 1" Copper Services to Property Line (w/ Curb Stop)
- New Hydrants
- New Valves



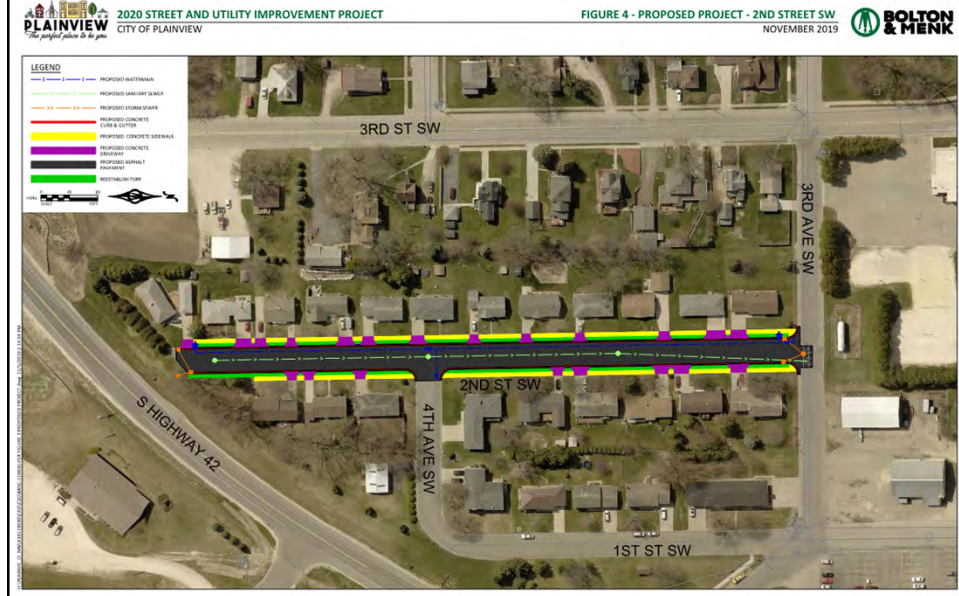
18

Proposed Conditions



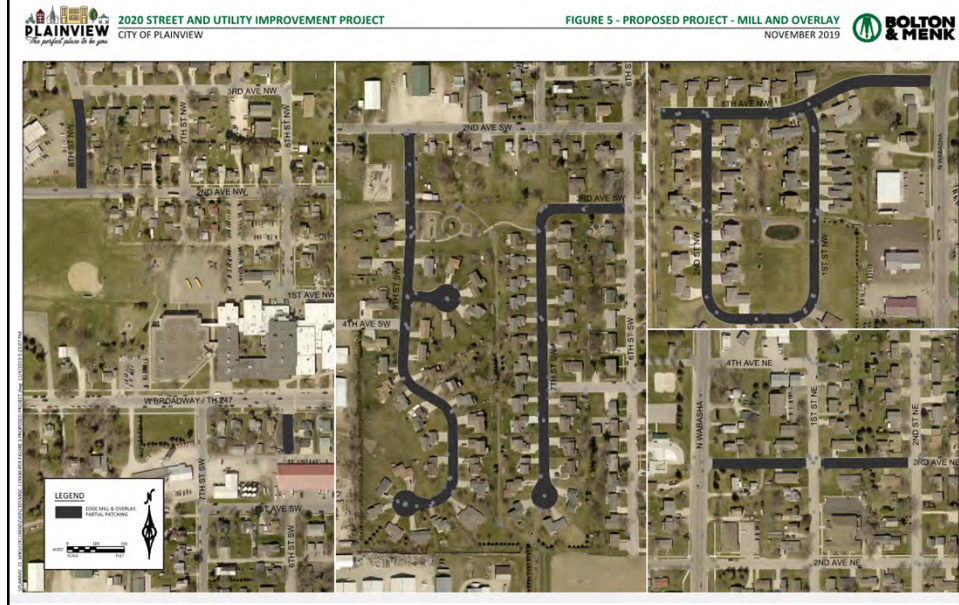
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Proposed Conditions



20

Proposed Conditions



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Other Utilities

- Communications (internet, phone cable)
- Natural Gas
- Electric
- Design to be coordinated with these systems



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Estimated Costs

Table 3 – Preliminary Cost Estimate			
Item	Estimated Construction Cost	Estimated Engineering, Administration, and Financing Cost	Total Estimated Project Cost
Hillcrest Subdivision – Base Project	\$ 1,204,505.00	\$ 301,200.00	\$ 1,505,705.00
2 nd Street Southwest – Alt. No. 1	\$ 677,307.50	\$ 169,400.00	\$ 846,707.50
Misc. Mill & Overlays – Alt. No. 2	\$ 694,315.00	\$ 173,600.00	\$ 867,915.00
Total Estimated Project Costs	\$ 2,576,127.50	\$ 644,200.00	\$ 3,220,327.50



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Financing

- CIP financial plan identified the ability to do a \$4 million project in 2020. Discussed splitting that into \$2 million each in 2020 and 2021.
- We understand project will be financed using a bond sale repaid with:
 - Utility Funds
 - Tax Levy



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Tentative Project Schedule Summary

Table 3 – Project Schedule	
Council Orders Preparation of Feasibility Report	10/8/19
Prepare Feasibility Report	10/9/19 - 11/8/19
Neighborhood Informational Meeting	11/6/19
Resolution Receiving Report	11/12/19
Resolution Ordering Improvement and Preparation of Plans and Specifications	11/12/19
Prepare Plans and Specifications	11/13/19 - 3/10/20
Neighborhood Informational Meeting	2/18/20
Resolution Approving Plans and Specifications and Ordering Advertisement for Bids	3/10/20
Advertise for Bids	3/19/19
Open Bids	4/9/19
Resolution Awarding Contract	4/14/20
Neighborhood Informational Meeting	4/28/20
Begin Construction	May 2020
End Construction	June 2021



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Neighborhood Feedback

- Neighborhood informational meeting held on 11/6/19
- Residents in reconstruction areas invited
- 13 signed in, most from Hillcrest area
- Same presentation as tonight – added discussion about construction expectations
- Primary feedback from meeting
 - Nearly all in attendance from Hillcrest area were not in favor of adding sidewalks



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Conclusions/Recommendations/Decisions

- Proposed improvements are necessary, cost effective, and feasible
- Need direction from the City on following items:
 - Extent of the project scope – Base Project + Alternates
 - Recommendation is to proceed with both reconstruction areas and mill/overlay projects
 - Need and ability to finance has been established, no advantage to waiting



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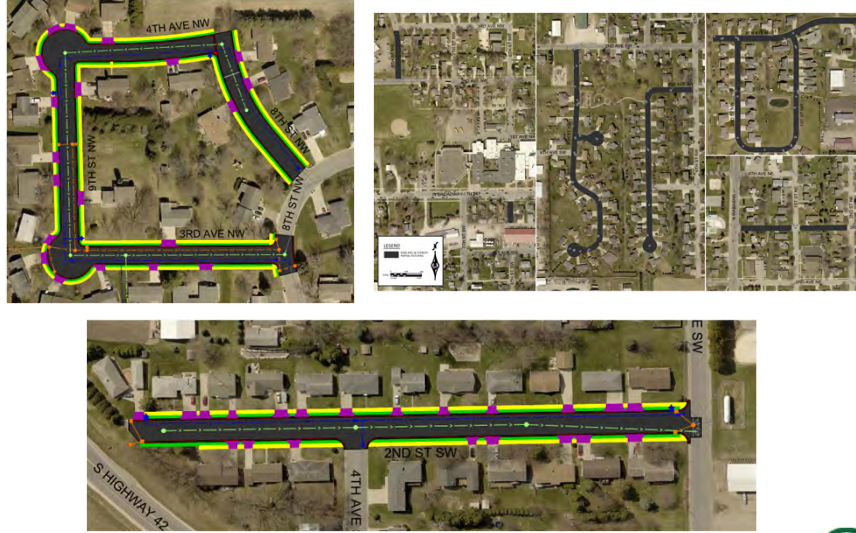
Conclusions/Recommendations/Decisions

- Need direction from the City on following items:
 - Should sidewalk be added to Hillcrest Second Subdivision?
 - Recommendation
 - Option 1 – Implement policy to add sidewalk to both sides of every street upon reconstruction.
 - Option 2 – Develop City-wide sidewalk plan, with community input. Refer to plan when reconstructing streets.
 - Key – If decision is no sidewalk in Hillcrest, need to develop City-wide plan.



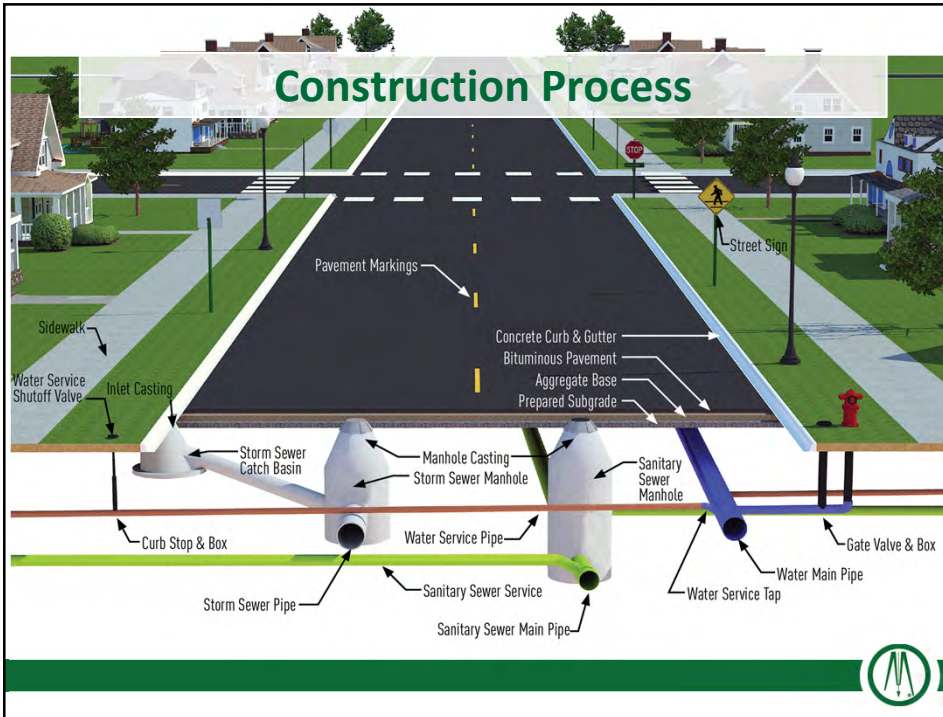
28

Questions?



29

Construction Process



30



31



32



33



34

Construction Issues: Garbage & Recycling



- Same Day, Time, Location
- Contractor will work with garbage company
- Contractor will return receptacles



35

Construction Issues: Mail Delivery



- Contractor will work with Post Office
- Temporary mailboxes
- Location will be provided in newsletter
- Remove/Reinstall



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Construction Issues: Water/Sewer Services

- Services replaced to R/W line
- Remaining service will be inspected, homeowner notified if replacement is required/recommended
- Replacement from R/W to home would be homeowner's responsibility
- Temporary Water



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Construction Issues: Driveways



- Concrete Apron
- Replacement
- Concrete
- Bituminous/Asphalt



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Construction Issues: Sidewalks



- Replacement
- Curing



39

Construction Issues: Turf Establishment



- Amended Topsoil
- Fertilizer & Seed
- Hydromulch
- 70% Growth
- Maintenance



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Construction Communication: Newsletter



2020 Street & Utility Improvement Newsletter

October 21, 2019

Project Schedule:

Planning - Fall 2019

Design - Winter 2019/2020

Construction - Summer 2020

Overview

The City of Plainview is beginning to plan for the 2020 Street and Utility Improvement Project. Currently, the following two areas are being considered for full street and utility (sanitary sewer, watermain, and storm sewer) reconstruction:

- Hillcrest Second Addition
- 2nd Street Southwest

The location map on page 2 shows the areas being considered in more detail.

The first steps in the planning process include the completion of a topographic survey and the preparation of a Feasibility Report. The topographic survey work is scheduled to begin the week of October 28, 2019.

A neighborhood informational meeting is planned for Wednesday, November 6, 2019 at 6pm at City Hall to present information from the Feasibility Report and inform interested residents about the project being considered.



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Construction Communication: Project Website

Coming Soon.....

City of Rushford – 2019 Street & Utility Reconstruction



City of Rushford

Project Links

Project Overview
Schedule
Contacts
Maps & Documents
Construction Updates

Project Overview

The City of Rushford is beginning to plan for the 2019 Street and Utility Reconstruction Project within the following areas:

- E Grove Street (Walnut Street to Money Creek Street)
- N Burr Oak Street (E Grove Street to Circle Drive)
- Walnut Street (E Grove Street to E Rushford Avenue)
- Lamplighters Lane (Street improvements only)

Improvements will include complete replacement of bituminous pavement, curb and gutter, watermain and services, and sanitary sewer mains and services. Underground storm sewer will also be extended into the project area. The estimated cost of the proposed improvements is \$1.825 Million. A portion of the project costs will be assessed to adjacent properties in accordance with Chapter 429 of the Minnesota Statutes.



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Construction Communication: RPR

- Bolton & Menk will have a Resident Project Representative on site every day construction is taking place.
- Question, Comments or Concerns should be directed to the RPR. Contact information will be provided.
- Limit questions to the contractor, staff isn't always completely in the loop on what is happening, RPR is.



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Before You Leave

- Provide name and contact information on sign-in sheet
- More information on specific property impacts will be determined during design phase. We have tentatively scheduled a second Neighborhood Informational Meeting on 2/18/20.
- Specific concerns now, let us know.



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